

# Schedule A - PD-09-2025

- General Notes**
- All dimensions are in metres unless otherwise noted. This drawing shall not be scaled.
  - All work shall be in accordance with the requirements of the local municipality, the latest relevant sections of the OPSS's, OPSD's, and the Ontario Building Code.
  - Soil Management Regulations: All import or export of soil related to this site is to be completed in conformance with Ontario Regulation 406/19: On-site and Excess Soil Management. Per the regulation, it is the responsibility of the owner to retain a Qualified Person (QP) to investigate and/or develop (or supervise the development of) a site-specific excess soil plan.
  - The Contractor shall obtain all necessary locates & permits prior to commencing work.
  - The Contractor shall notify the Engineer 24 hours prior to constructing any works in order to coordinate inspections.
  - The Contractor shall, at their own cost, install and maintain erosion control measures for the duration of construction, in accordance with local and provincial regulations or as directed by the Engineer.
  - Only drawings stamped "Issued for Construction" shall be used for construction.
  - All embankment slopes are at maximum 3:1, unless otherwise shown.
  - Proposed grades are to match existing grades at the perimeter of the work site, unless otherwise shown.

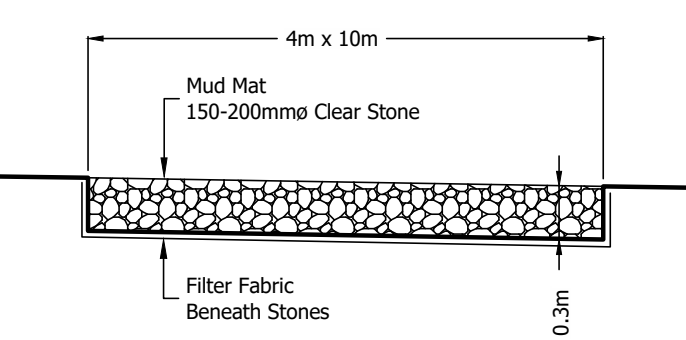
- Erosion and Sediment Control Strategy**
- The Contractor shall attend pre-construction meeting with agencies and provide approved final Erosion and Sediment Control Plan a minimum of one week prior to construction.
  - Any deviations from the approved strategy must be approved by the Contract Administrator and Engineer.
  - Newly constructed banks to be seeded per specifications.
  - All materials and equipment used for the purpose of site preparation and project completion should be operated and stored in a manner that prevents any deleterious substance (eg. petroleum products, silt, debris, etc.) from entering the water.
  - Once grading in a particular area is complete, that area shall be revegetated per plans.
  - Erosion and Sediment controls are to be removed at the completion of the project after as joint inspection by the Contractor and Engineer.

- Standard Environmental Measures Notes:**
- Every reasonable attempt must be made to minimize construction related disturbance to a watercourse.
  - Refuelling activities shall be conducted in an environmentally responsible manner. This includes maintaining a 30m setback from the water edge, drainage pathway or unless otherwise directed by the Contract Administrator. Spill kits and sufficient amount of sorbent material shall be available on the fuel or service vehicles.
  - Any part of equipment entering the active watercourse shall be free of fluid leaks and externally cleaned and degreased to prevent any deleterious substances from entering the water.
  - Any spills resulting from refuelling operations, hydraulic leaks, maintenance etc. must be reported immediately to the Contract Administrator who shall then notify the Spills Action Centre as required.
  - Weather conditions shall be monitored to adequately prepare the site for rain events.
  - Cleanliness of adjacent streets shall be maintained at all times.

- Area Grading Implementation Schedule**
- Install all silt fencing per OPSD 219.130, as shown on plan.
  - Construct mudmat on Young Street, per detail.
  - Remove trees as per plan.
- Erosion and Sediment Control Notes**
- All works to be done in accordance with OPSS 805.
  - All silt fence to be installed prior to commencement of any area grading, excavating or demolition, unless noted otherwise.
  - Erosion control fencing to be placed around the base of all stockpiles. All stockpiles to be kept a minimum of 5.0m from all property lines. A 5m maintenance strip must be maintained around all stockpiles (between the stockpile and the fencing).
  - Additional erosion control measures may be required as site development progresses. Contractor to provide all additional erosion control structures in accordance with the contingency allowance.
  - The Engineer shall monitor the site development to ensure all erosion controls are installed and maintained to the municipal requirements, and any damage repaired immediately. Contractor to comply with the Engineer's instructions to install, modify, or maintain erosion control works. Sediments to be removed when accumulations reach a maximum of one third (1/3) the height of the silt fence.
  - All erosion control structures to remain in place until all disturbed ground surfaces have been re-stabilized either by paving or restoration of vegetative ground cover.
  - No alternate methods of erosion control protection shall be permitted unless approved by the Engineer and the municipality.
  - The contractor is responsible for removing sediments from the municipal roadway and sidewalks at the end of each work day.
  - Sediment traps to be provided on site at all locations where construction vehicles exit the site. Sediment traps shall be a minimum of 4.0m wide, 10.0m long and 300mm deep and shall consist of 50-150mm angular rip rap material or approved equivalent. Contractor to ensure all vehicles leave the site via the construction access and that the sediment trap is maintained in a manner to maximize its effectiveness at all times.
  - Areas affected by grading activities shall be topped (125mm minimum thickness) and seeded within 30 days of site activity ceasing.
  - Excess fill material shall not be disposed of within environmentally sensitive areas, including wetlands, woodlots, regulated areas, or adjacent properties.
  - The property owner is responsible for restoration of all damaged and/or disturbed property within the municipal right-of-way to the municipal standards.
  - If, for unforeseen reasons the Owner and/or his representative must encroach onto private lands to undertake any works, he/she must obtain written permission from the adjacent property owners prior to entering upon the private property to perform any works. Copies of these letters of consent must be submitted to the municipality, prior to any work being performed. Failure to comply with the above is at the owners own risk.
  - Monitoring and weekly inspection reporting per the municipal requirements.

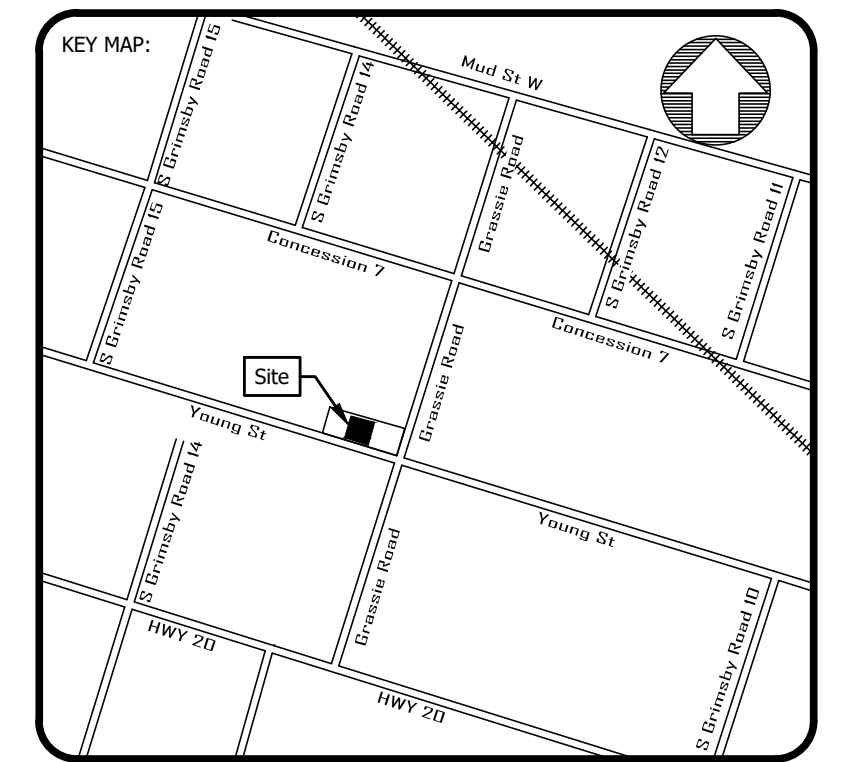
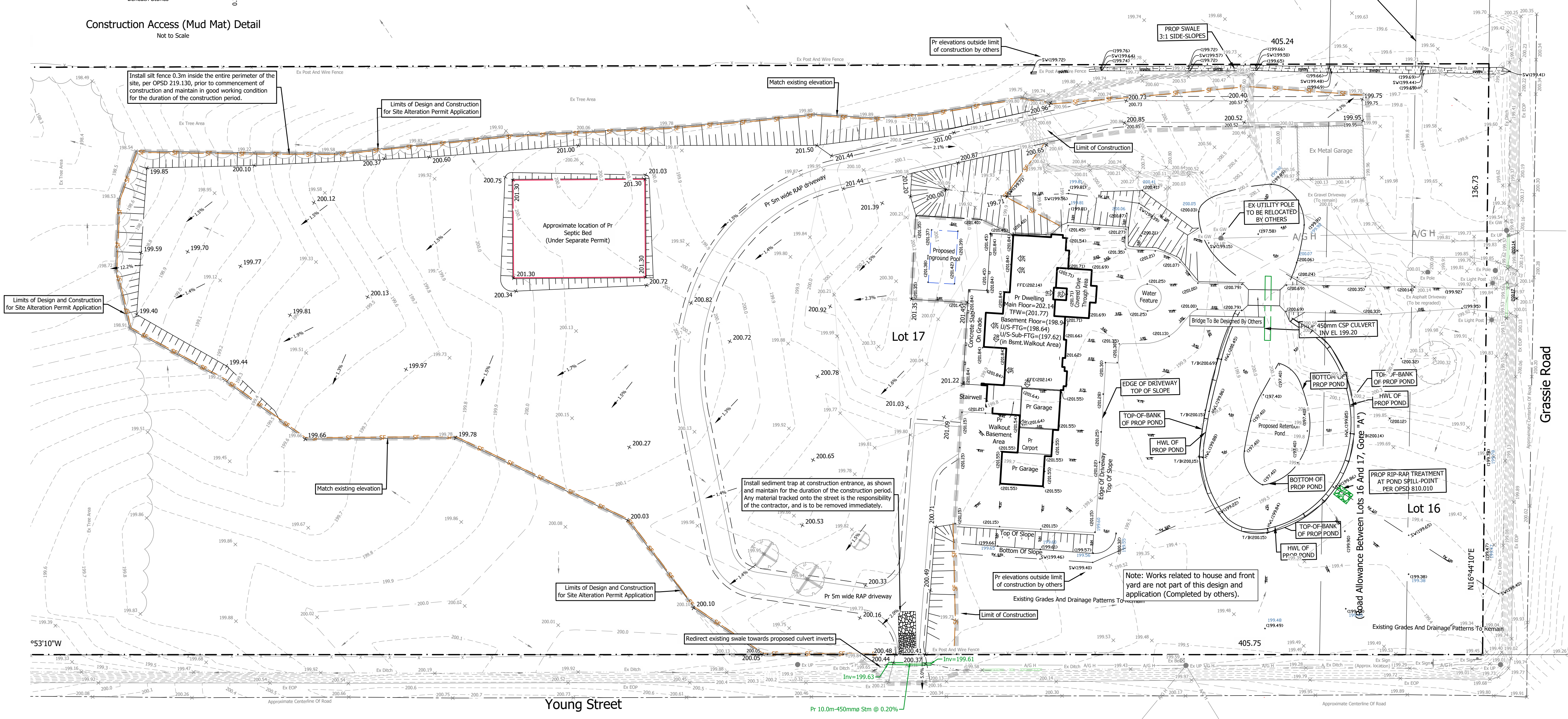
Approximate Fill Volume = 10600m<sup>3</sup>

Designated Access for All Construction Traffic. Install "Mud Mat" per detail below, prior to any other construction. Mat to be maintained in good working order until grading works are completed and granular "A" & "B" have been placed.



Construction Access (Mud Mat) Detail  
Not to Scale

- Grading Legend**
- (201.15) Design elevations by AHA
  - ✕ 200.71 Proposed elevations for Site Alteration Permit
  - ✕ 200.71 Existing elevation



- Project Notes**
- This drawing is to be read with the standard notes, specifications and details shown on Meritech dwg 5311-1.
    - Front site grading design by Ashenhurst Nouwens & Associates Inc., dated Sep, 2023
    - Survey and elevations:
      - Topographic survey completed by Ashenhurst Nouwens & Associates Inc., dated Oct, 2022
      - Benchmark: Township of West Lincoln Benchmark No.0011985U176. Elevation = 197.115 (CGVD 28-78) Hamilton-Wentworth iron pipe with brass cap, at intersection of Mud Street with County Road No.50, in line with power poles 29.3 North of centre line of Mud Street, 8.2m West of centre line of County Road 50, 21.6m North of hydro pole No.2190, 15cm East of benchmark sign post, at road level.

(Road Allowance Between Lots 16 And 17, Gore "A")  
Conveyed By By-law 7-71,  
Inst. RO219141

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DRAWING	DESIGNED BY	CHECKED BY	DATE	FILE NAME	SCALE	SHEET
Grading Plan	JAS	BRE	Dec 23, 2024	5311	1:500	1 of 1

OWNER: **Town of West Lincoln**

LOCATION: **3118 Grassie Road**

PROJECT: **Part of lots 16&17 and Part of road allowance Lot 16&17 Gore A, Town of West Lincoln**

ISSUED FOR: **Site Alteration Permit**

DATE: **Dec 23, 2024**

REVISION/ISSUE: **1.**

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